

# THE IRONMAN—A DOUBLE ENDER’S NEWSLETTER

**Membership Scorebox**

Active	143
Lifetime	52
Current Members	40
Dues paid	
Need to Renew	46
Snail Mail’s Sent	43
Email Address	505
Total Shipmates	4,252

## USS Reeves Legend Series GMMC Robert E. Neuhaus – USN (Ret) USS Reeves Plank Owner By Gerry Hines FTM2 VP USS Reeves Association



**Dues Notice!**  
You should renew your annual dues. To reach the greatest number of shipmates, we will publish the complete Association newsletter to any valid email or snail mail address. Your dues payments make this possible.

This will be another installment of our Reeves legends series. A short time ago we got word that Robert Neuhaus GMMC had passed away last January at the age of 88. Robert was a plank owner on the Reeves but that is only part of his over twenty-year Navy career. Through his obituary notices I was able to contact his son James, who provided most of the information for this article. While attending the boot graduation ceremony of his nephew at Great Lakes one of the chiefs at the ceremony told James that Robert had been one of the “Legends” that taught at the gunner’s mate school there. That certainly ties in with our legend’s series. James tells me that Robert never talked about his Navy years until recently when he would tell his grandson and newly recruited nephew about his experiences. I will try to research some of the details of those stories.

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Robert was born in Dorchester, WI. In 1934. His father was a farmer. Robert was one of 12 children. All four of the boys in the family served in the military split between the Army and Navy. Interestingly, while they didn’t serve in the military Robert’s son James and grandson Austin both work as first responders. A family tradition of serving this country.

He joined the Navy in March of 1951, went to boot camp in San Diego and then was assigned to the cruiser USS Rochester CA-124. The Rochester was involved in combat operations off the coast of Korea right up until the end of the conflict. She was known as “The Grey Ghost of the Korean Coast” firing many thousands of rounds in support of shore operations. During Robert’s first West Pac tour, which included the winter of 52-53 spent off the east coast of Korea. Rochester fired over 7000 rounds.

## The President’s Page



Happy New Year 2024 to the entire USS Reeves Association Family. Once again it seems incredible that another year has passed us by. Time seems to be accelerating. It doesn’t seem all that long ago we were just writing the newsletter articles on the very successful Reeves Reunion in Dayton, and here we are in 2024.

I think that from an association perspective, 2023 turned out to be a pretty good year. We started last January by looking forward to getting our shipmates to attend a reunion at a time when the cost for practically everything was going straight up. The less-than-ideal economic conditions were discouraging. But we were committed and never backed away from our efforts to propel everyone to Dayton. We ended up being amazingly successful in a year where the economy shouted, “stay home and save your money”. Proof yet again that our shipmates were not to be swayed by dismal news when there was an opportunity to meet with their Reeves shipmates and friends.

We also tentatively set the stage for the 2025 reunion, and we’ll put together our battle plan over the next few months to get us into the game for October 2025. That includes resetting our Request for Proposal (RFP) document to address our next target. So, for at least the next few months, I’m happy to not be concerned with the ’25 reunion. We’ll see how that goes...

My attention now turns to the health of the Association. We are ahead of the game money-wise, even after having spent a little bit more than we budgeted for Dayton. The costs for just about everything ramped up after we determined what we would need to see our plan through. There was really no way to

predict some of those cost escalations, but thanks to the generous and unsolicited donations from our shipmates and family in Dayton, we made it through nicely. We can’t thank you enough for your generous donations and support that really saved the day.

Another success in Dayton was the performance of the Ships Store, as sales were excellent due to a generous crowd loading up on Reeves bling. The ships store is essentially self-sustaining under normal conditions, and a money maker during peak reunion sales conditions. Kurt has managed the store very efficiently and had everything we wanted in stock at the reunion. He was also able to move some of his budget surplus over to our general fund after the reunion. The ships store has been restocked since the reunion rush, so we are ready with sufficient inventory to support on-line sales.

We made a commitment at our business meeting to get the website up to speed in the new year. That hasn’t exactly taken off, but we’ll plead our case that we’re only a couple of weeks into the new year, and that there is some time to get our arms around the problem. I’ll track down Dave Samuels and see if we can get started soon. It just needs some time and expertise. We’ll find both and get the site up to speed.

The next order of business to celebrate our 18<sup>th</sup> year of being an official association. Our job now is to continue the task of keeping the Reeves alive in our hearts, and continually promoting the brotherhood that that wonderful ship left for us to carry on as her legacy. Gone but not forgotten is only one aspect of the task. Keeping in touch with our shipmates and reunion family is most important in a year when there is no reunion planned. We have to make sure that we keep an eye on each other and be good shipmates

*(Continued on page 6)*

**This newsletter is published by:**  
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All comments, suggestions, submissions and criticism are welcome. My email is always open..

## Life Memberships

At the 2014 reunion in Portland, shipmates voted for a Life Membership. After researching various Associations, it was decided that the US Navy Cruiser Sailor Association’s plan was best for the USS Reeves Association. Monies for life membership are placed in an interest-bearing account and not mixed with operating funds.

The schedule is simple:

<u>Age</u>	<u>Amount</u>
Under 50	\$500
Under 60	\$400
Under 70	\$300
Under 80	\$200
80 or older	\$100

We already have 52 life members.

## USS Reeves Legend Series (cont)

*(Continued from page 1)*

In January of 1953 Rochester made a goodwill stop in Saigon. She was the largest ship to ever visit the city. The Rochester drew 26 feet, and the river was 26 ½ feet at high tide. On arriving at Saigon, the river was narrow and in order to turn the ship around, they had to run the bow into the bank of the river and push the stern around with tugs. The OOD mentioned in his log entry that he was probably the only one to ever make an entry that to turn the ship around it had to be run aground.

Robert was aboard the Rochester until 1961 when the ship was decommissioned. While on board he went through 5 West Pac cruises including 2 of those trips up to Saigon, one to Sidney, Australia, and many stops in Japan.

September 1961 to April 1963 he was assigned to the Naval Missile Center at Naval Air Station, Point Magu, CA. Transitioning from the gun to the missile Navy. Another year was spent at Great Lakes for more schooling before reporting aboard with the Reeves Pre-commissioning crew in March of 1964 at Bremerton, WA. He reported aboard as a Gunner's mate first class. His time on Rochester included a lot of at sea time. After commissioning and sea trials Reeves was no different. Leaving for West Pac and Viet Nam in early 1965 she returned to Long Beach and a few months later transferred to Yokosuka and many trips back and forth to the Tonkin Gulf. He told his son about having a fear of fire because of a fire aboard ship but didn't tell him which ship. It was Reeves, about 2 days out of Subic on the way to Northern SAR station. The fire was discovered by the roving patrol at about 2200 in the power room of the aft Terrier System Radar. Smoke billowing out into the night, it was a very spooky sight. The power room was directly above the 3" 50 magazine and part of the deck actually melted. No wonder Robert retained a fear of fires. It was during this time (1966-68) that Robert added another star to his combat action ribbon. We were close in to North Viet Nam supporting the helicopter rescue of a downed pilot. The chopper came out at us like a bat out of Hell with flak following him. USS Blue DD-744 who was escorting us took out the gun emplacement with her 5" guns. We turned and scrambled out of there. The pilot was air lifted back to the carrier.



Robert remained aboard Reeves until 1969 when she was taken through the Panama Canal on the way to Bath, Maine for extensive renovations. He was transferred back to Great Lakes to teach at the Gunners Mate School remaining there from July 1969 until June of 1972. After all his years of experience at sea with the Terrier Missile System he would have been highly qualified to teach it. His last duty was aboard the USS Worden DLG-18. He reported aboard in July of 72. Worden was home ported in Yokosuka and would have been quite familiar to Robert after his Reeves experiences over there. Worden had been the victim of a friendly fire incident only 4 months earlier when 1 sailor was killed and 8 injured. The ship was constantly going back and forth to the Tonkin Gulf doing the same kinds of duty that Reeves had been involved with. Plane guard (They did pick up a flight deck crewman who had fallen overboard from a carrier) and Northern SAR picket duty. Robert was aboard when on January 28, 1973, the Vietnamese Cease Fire was announced.

With a career spanning from the last years of the Korean war to the end of the Vietnam war Robert retired into civilian life. A career of just short of 22 years.

After that full career in the Navy, it was time to start a family. In 1976 he married Ruth Wessolowski. They had two sons Michael and James and a grandson Austin. He worked for over 20 years as head of building

*(Continued on page 4)*

## USS Reeves Legend Series (cont)

*(Continued from page 3)*

maintenance for Andre's Steak House in Richmond, Illinois. Despite his fear of fires, he saved a man's life. An electrician who got careless wiring a panel which exploded in a fireball in his face. Robert entered the burning electrical room and dragged the unconscious man to safety, receiving a few minor burns himself. Never one to draw attention to himself, he refused to go to the hospital.

James found a small notebook where Robert wrote about some of his port visits and some poetry, and the wording proves that even as an unassuming quiet man he was a true sailor. A poem he wrote follows to end this article.

### A Sailor's Life is Swell

I'm sitting here thinking, of the days I've left behind,  
 And I'm putting down on paper what is running through my mind,  
 People on the outside rate, a sailor's life is swell.,  
 But I'm here to tell you mate, a sailor's life is hell.  
 He has one consolation though, gather round and I will tell.  
 When I die, I'll go to Heaven, cause I've done my stretch in Hell.  
 I've scrubbed a thousand bulkheads, chipped a million miles of paint.  
 A meaner place of Hell, I'll swear to you there ain't.  
 I've stood for endless hours, just waiting for my mail  
 I've stood a million watches and been in every special detail.  
 I've shined a hundred miles of brass and I've scrubbed my dirty deeds.  
 I've swung ten thousand hammocks, and I've peeled two million miles of spuds.  
 But when the last taps are sounded, and I lay aside life's cares.  
 I'll take my final shore leave right up those golden stairs.  
 Tis then Saint Peter will greet me, and loudly he will yell,  
 take a front seat in Heaven sailor cause you pulled your share in Hell.

### Letter to the Editor

January 2, 2024

Dear Fumiyo,

I wanted to tell you how much I enjoyed the Origami class you taught at this year's reunion in Ohio. You were so patient with us and you made it a lot of fun entertaining us during the men's business meeting. Thank you so much.

I hope you had a very nice Christmas with your family and that 2024 will be a happy, healthy year for you.

Sincerely,

Janet Hines

## Bidding Farewell to LCDR Woodie C Stegall USN (Ret)

We are sorry to announce that Woodie C Stegall passed from this life on November 8th, 2023, at age 95. Woodie was a true “USS Reeves Legend”, as he was not only a Reeves Plank owner, but had the honor of being Reeves’ very first Navigator. He was also the subject of the very first Reeves “Legend Series” feature articles, which convened in the Spring 2023 Issue of the Ironman Newsletter. We could not think of no one better suited to have had the honor of being the first of the legends. We are sorry that we were not able to announce this news earlier, but we just got notified by his wife, Claire, who has undoubtedly had a very busy month. We thank Claire for taking care of our shipmate over the 73+ years of their marriage. We share her loss and pray that we will all get to see Woodie one day on Fiddlers Green.

We thank you, LCDR Stegall, for your service to our country, and for being a true legend to your USS Reeves Shipmates. May you Rest in Peace, Sir.



## Financials

### Income

Current Checking	4792.66
Current Savings	8493.50

*(Continued from page 2)*

when we know that we're at least 18 months away from our next reunion. Be a shipmate and stay in touch.

Here's wishing you fair winds and following seas as we glide into 2024. We hope it ends up being a healthy and Happy New Year for all of us.

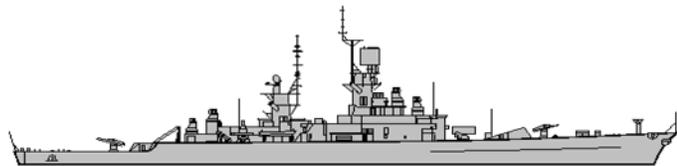
Regards

//tom

Tom Bailey, B-Div 84-87

### **Charitable/Educational Objectives**

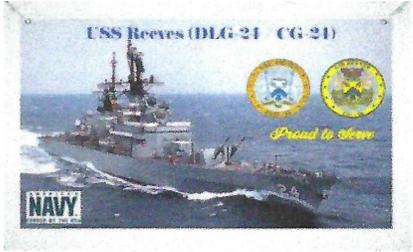
The USS Reeves Association is an educational, non-profit 501(c)(3) organization registered with the Internal Revenue Service, FEIN 86-1163983. For that purpose at the 2010 business meeting, it was recommended that future excess funds be used for donations and scholarships as determined at business meetings starting in 2012.



## Shift Colors

To receive the Navy's Shift Colors newsletter via email send the following information to [Mill\\_ShiftColors@navy.mil](mailto:Mill_ShiftColors@navy.mil) -- First name; Last name; E-mail address; Title (Mr., Mrs., rank, etc.); and Military affiliation (retiree, surviving spouse, veteran, other). There is no longer any funding for hard copies to be printed and mailed. To receive Shift Colors it does not matter whether or not you're retired, active duty, a veteran, a surviving spouse, or just someone who's interested in receiving the newsletter.

## Ship's Store

<p><b>Photo #1 1967 11" x 17" \$10 + s/h</b></p> 	<p><b>Embroidered Golf Shirt \$20 + s/h 2X and 3X add \$2.00</b></p> 	<p><b>Reeves HD Vinyl Banner, 30"x48" \$30. Each + \$14. s/h</b></p> 
<p><b>Photo#2 1972 11" x 17" \$10 + s/h</b></p> 	<p><b>Embroidered T-Shirt \$15 + s/h 2X and 3X add \$2.00</b></p> 	<p><b>Reeves 2" custom acrylic coated Challenge Coins \$14. ea +\$3.50 s/h for 1st coin &amp; \$.100 ea for extras. Limit 3 coins</b></p> 
<p><b>Photo#3 1986 11" x 17" \$10 + s/h</b></p> 	<p><b>DLG or CG Ball Caps \$12 + s/h</b></p> 	<p><b>Pens and 2 1/2" DLG or CG Vinyl Stickers *</b></p> 

### USS Reeves Association Ship's Store Order Form

Item	Description / Size	Qty	Price	Shipping Handling	Total
<b>Photos</b>	Specify Photo# 1, 2, or 3			\$2.00 each	
<b>Ball Cap</b>	Specify DLG or CG			\$5.00 each	
<b>Vinyl Banners</b>				\$14.00 each	
<b>Challenge Coins</b>	Limit of three			\$3.50 / 1.00 for addl	
<b>Golf Shirt</b>	Sizes run large			\$6.00 each	
<b>T-Shirt</b>	Sizes run large			\$6.00 each	
<b>Ball point pens</b>				\$1.00 each	
<b>Vinyl Stickers</b>	*Specify DLG or CG			\$1.00 each	
				Total	

\*Pens and stickers are available only with other items purchased

Shipping is via USPS Priority.

**Checks Only**, please make payable to:

Kurt Stuvengen  
 410 E. Spring St  
 Orfordville, WI 53576  
 Cell: (608) 921-5586

Name: _____
Address: _____
City / State / Zip: _____
Phone#: _____ E-Mail: _____

## Good News from the Upper Left

I recently got an opportunity to have a phone conversation with Michael Robertson. It had been a while since we had spoken, and I was entirely surprised by the conversation. Mike sounded far better than the last few times I had spoken with him. His voice was stronger and clearer, and his attitude was back to the old Michael than I have heard in years. Mike is in remission from his bouts with Cancer and his health is greatly improved. He told me he has purchased some new comms gear (cell phone) and a new iPad, and that he is back online doing some of the things he has not enjoyed for a few years. His main interest these days is tracing genealogy, and I could tell by his excitement that he's happy to be back in the game. I invited him to write a few articles for the Newsletter, and explained how we missed his "Ruminations from the Northwest" columns. He made no promises, but I'm hoping we'll hear from our founder and long-time leader yet again. The articles may just be on the subject of genealogy, but we'll take what we can get as long as we have Michael back in our fold. Thanks, Mike, for eluding Cancer, and Lorrie has our gratitude for returning our friend and shipmate back to good health.

## Christmas in Korea 1981



The past Christmas season reminded me of an incident that happened in the early 80's. While it ended quickly and peacefully it brought into perspective of just why we were out there at the "Tip of the Sword".

The month of December 1981 found Reeves underway once again. We were escorting the USS Midway (CV-41) down to Sasebo in southern Japan. After a couple of weeks steaming and conducting exercises in the waters off Kyushu we pulled into Sasebo on December 12. The plan was to enjoy some liberty before heading home to Yokosuka for the holidays. On the 19<sup>th</sup> the ship received word of political and/or military unrest on the Korean peninsula. The ship did an emergency sortie out of Sasebo heading to Korean waters for contingency operations. Of course, the whole time we were up off Korea the rumor mill was up on the governor with what ifs. Word on the deck plates was we wouldn't be home for Christmas and might not be back in Yokosuka until the New Year. This led BTCS Hawker to comment at quarters, "if we can't be with our families for the Holidays we may as well be with our loved ones". The picture is of BT3 Wesley Hill and the Christmas tree he made for After Fireroom.

Fortunately, things quieted down quickly, and we arrived back in Yokosuka late on the 23<sup>rd</sup> of December so the crew was able to enjoy Christmas with family and friends.

Kurt Stuvengen

## Red Sea Hostilities



*The above photo of the USS Reeves was taken in 1987 at the height of Operation Earnest Will, as Reeves escorted a re-flagged tanker through the Straits of Hormuz. Those were dangerous days, as Iran continually threatened and attacked both international shipping and warships in the region. Fast forward 37 years, and it appears that nothing has changed, except for the escalation of Iran backed forces into the Red Sea. The USN of today is already fully engaged in maintaining freedom of the sea lanes in the region and has shot down numerous missiles and drones aimed at commercial shipping. It appears, according to the Reuters Article copied below, that Iran is about to ramp things up. Keep in mind that the majority of our sailors who are currently in harm's way in the Red Sea were not even born when Reeves proudly served the same role. Please keep our Naval forces in your prayers as the tip of the spear spirals into an inhospitable 2024.*

## Iran Vows To Target More Enemy Ships in Global Waters

January 6, 2024



by Jan Harvey (Reuters) The commander of Iran's Revolutionary Guards vowed on Saturday to reach "the enemy" far and near as tensions soar on key shipping routes where Tehran's allies have been attacking vessels. "Today, we are facing an all-out battle with the enemy," said Guards commander Hossein Salami at a ceremony in the southern Gulf port city of Bandar Abbas, where the Guards' navy unveiled a new ship named "Abu Mahdi" and 100 missile launchers.

Salami did not name the enemy, but nations have agreed to participate in a [U.S.-led coalition to safeguard commercial traffic in the Red Sea](#) from attacks by Yemen's Iran-backed Houthi movement. The Houthi attacks since November are a show of support for the Palestinian group Hamas in its war with Israel.

In response, many major shipping companies have switched to the longer and more costly route around the Africa's Cape of Good Hope rather than pass through the Suez Canal, which handles about 12% of global trade.

"We need to defend our national interests to wherever they extend," Salami said in a televised speech. "It will be harmful for the enemy to be found near and at a half distant. They should stay away from this area." The Guards' navy, he said, had made a "brilliant leap in its offensive and defensive powers" to challenge the world's naval powers.

According to Iranian media, [Iran's Alborz warship entered the Red Sea earlier this month](#) to secure shipping routes.

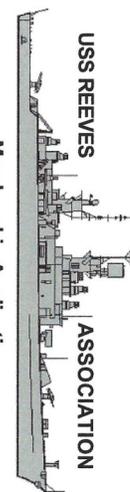
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**USS Reeves Association**  
 1154 S Greenway Ave  
 Pueblo West, CO 81007-1745  
<http://www.ussreeves.net>

First Class Postage

**ADDRESS SERVICE REQUESTED**



**Membership Application**

Name \_\_\_\_\_  
 Street Address 1 \_\_\_\_\_  
 Street Address 2 \_\_\_\_\_  
 City/State/Zip \_\_\_\_\_  
 Home Phone \_\_\_\_\_  
 Cell Phone \_\_\_\_\_  
 Email Address \_\_\_\_\_

I served on the USS REEVES (DLG-24/CG-24) as a \_\_\_\_\_ (rate/rank) from 19 \_\_\_\_ to 19 \_\_\_\_ . Division: \_\_\_\_\_

Membership in the Association includes the right to vote and hold elective office. It also includes receiving the "Ironman Newsletter, email updates and further privileges as to be determined by the Board of Directors. We kept the dues at \$20.00 per year but have recently changed the duration to match our two-year reunion cycles.

*Dues: \$40.00 for two years*

I also wish to support the goals of the Association which is the building of a membership that will increase interest in bi-annual reunions, as well as the establishment of a newsletter and other means of communicating our activities with all Reeves shipmates. I am making an additional donation of \$ \_\_\_\_\_.

I enclose my check or money order in the amount of \$ \_\_\_\_\_.

I am interested in helping the Association grow and become successful. I wish to serve on a committee, provide other assistance or have an interest as follows:

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

Please mail application and check to:

**USS REEVES (DLG-24/CG-24) ASSOCIATION**  
 1154 S Greenway Ave  
 Pueblo West, CO 81007

For further information call or email Tom Bailey at (719) 647-2872  
[tombailey@ussreeves.net](mailto:tombailey@ussreeves.net)

Or find more details online at: <http://www.ussreeves.net/association.html>